



City of
Peterborough

To: Members of the General Committee

From: Blair Nelson, Commissioner, Infrastructure, Planning and Growth Management

Meeting Date: May 4, 2026

Report: Ashburnham Drive Realignment and McFarlane Street Bridge Replacement Class EA Approval, Report IPGACP26-005

Subject

A report to approve the recommendations of the Municipal Class Environmental Assessment for the Ashburnham Drive Realignment and McFarlane Street Bridge Replacement.

Recommendations

That Council approve the recommendation outlined in Report IPGACP26-005, dated May 4, 2026, of the Commissioner of Infrastructure, Planning and Growth Management as follows:

- a) That the presentation of Paradigm Transportation Solutions Limited on the matter of the Municipal Class EA for the Ashburnham Drive Realignment and McFarlane Street Bridge Replacement be received;
- b) That the recommendations of the Municipal Class EA for the Ashburnham Drive Realignment and McFarlane Street Bridge Replacement be endorsed; and
- c) That staff be directed to post the Environmental Study Report for the Municipal Class EA for the Ashburnham Drive Realignment and McFarlane Street Bridge Replacement for the 30-day review period and Provincial approval.

Executive Summary

The Ashburnham Drive Realignment and McFarlane Street Bridge Replacement Class EA was undertaken to support future development needs within the east portion of the City and the replacement of deficient roads and bridges that do not meet present-day needs and design criteria.

A Municipal Class Environmental Assessment (EA) was completed to identify a preferred solution that balances transportation needs, technical feasibility, environmental considerations, property impacts, and public input. The study followed a transparent and consultative process and resulted in a recommended road realignment and bridge replacement that addresses the identified deficiencies while supporting long-term mobility and safety objectives. The Environmental Study Report (ESR) documents the planning process, evaluation of alternatives, consultation activities, and the recommended undertaking.

Background

The Ashburnham Drive Realignment and McFarlane Street Bridge Replacement project emerged from the recommendations of the East Side Transportation Study (ESTS), which was completed in 2022. The ESTS assessed existing and future transportation infrastructure needs and priorities within the East City area and identified a series of strategic improvements required to support safety, mobility, and long-term network performance.

Based on the findings of the ESTS, the realignment of Ashburnham Drive, generally between Maniece Avenue and McFarlane Street, along with the replacement of the existing McFarlane Street bridge crossing the Trent–Severn Waterway, was recommended for implementation. The ESTS was undertaken in accordance with Approach #1 of the Municipal Class Environmental Assessment (MCEA) Master Planning Process, which generally satisfies Phases 1 and 2 of the MCEA process (Problem/Opportunity Identification and Alternative Solutions). Building on the ESTS recommendations, the City proceeded with Phases 3 and 4 of the MCEA process for the Ashburnham Drive Realignment and McFarlane Street Bridge Replacement EA.

A fundamental principle of the MCEA process is meaningful engagement with Indigenous Peoples, the public, review agencies, and other stakeholders. Opportunities to share information, provide input, and ask questions were provided throughout the study through formal notices, study updates, and public consultation events.

The Ashburnham Drive Realignment and McFarlane Street Bridge Replacement EA was formally initiated with the issuance of a Notice of Study Commencement on September 1, 2022. Notification included direct mail to potentially affected property owners, newspaper advertisements, and postings on the City's website to ensure broad public awareness of the study.

Key tasks completed as part of the EA included background data collection and review, confirmation of the problem and opportunity, development and evaluation of alternative road alignment and bridge design concepts, assessment of potential impacts to the natural, social, cultural, and built environments, identification of mitigation measures, and ongoing consultation with Indigenous communities, agencies, stakeholders, landowners and the public. The outcomes of these tasks are documented in the ESR, which accompanies this report as an appendix.

Problems and Opportunities

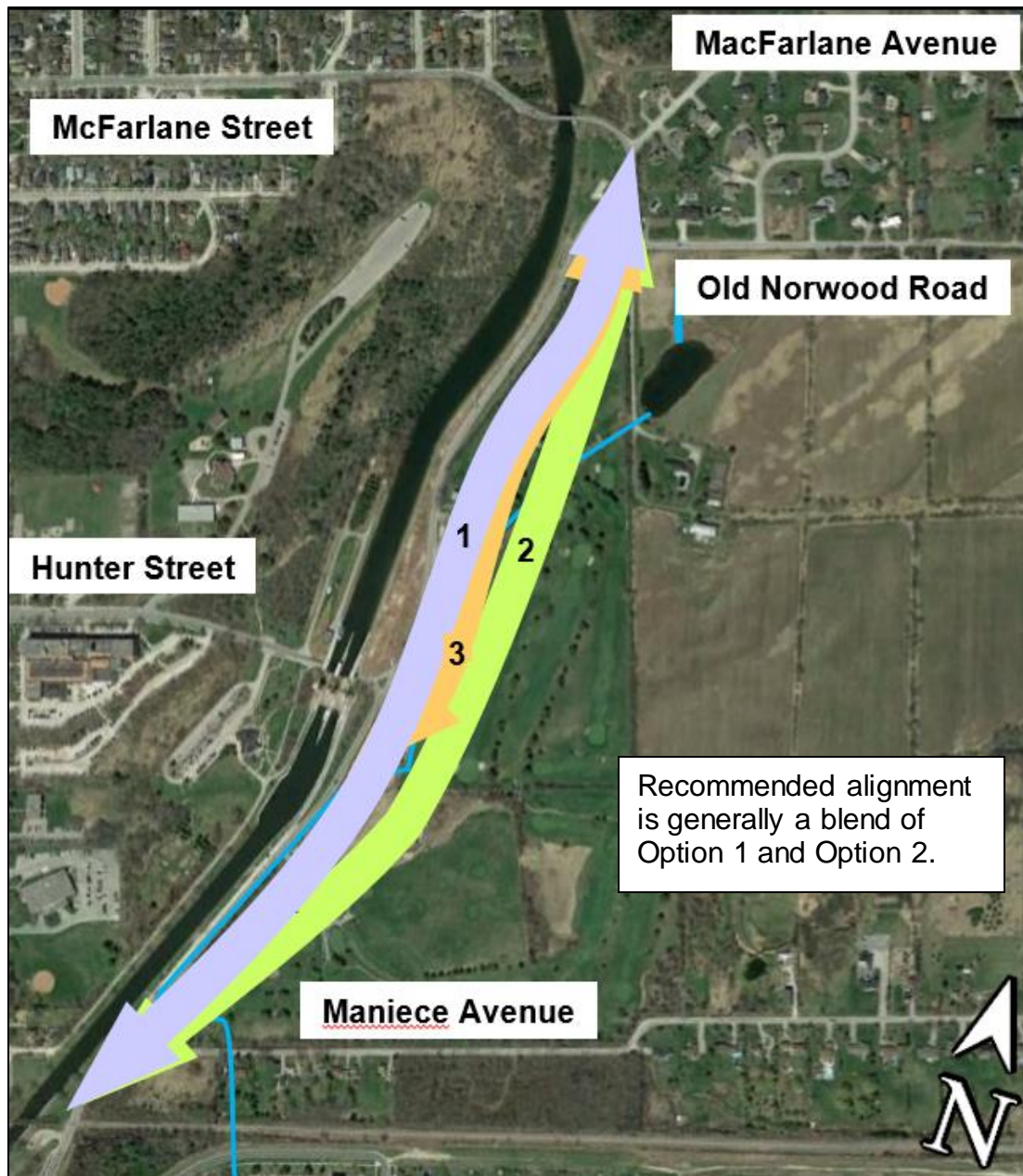
As part of the ESTS, several transportation deficiencies and opportunities were identified along Ashburnham Drive and at the McFarlane Street bridge. Ashburnham Drive runs along the east side of the Trent Severn Waterway, an area rich in leisure and tourist activities. However, the corridor lacks continuous sidewalks and cycling facilities, has limited parking provision, and is constrained by poor roadway geometry and sightlines, particularly north of the Lift Lock where the road is situated within the canal berm. This presents a significant challenge to planning physical improvements to the existing rural roadway and limits opportunities to enhance active transportation and transit access within the existing alignment.

The McFarlane Street bridge is currently limited to a single travel lane and does not provide dedicated facilities for pedestrians or cyclists, despite being frequently used by cyclists to access other parts of the City. The realignment of Ashburnham Drive and replacement of the McFarlane Street bridge present an opportunity to address these deficiencies by improving roadway geometry and sightlines, enhancing safety and capacity for all users, providing dedicated active transportation facilities, and strengthening connectivity across the Trent–Severn Waterway while supporting broader transportation, recreation, and tourism objectives.

Ashburnham Drive Realignment Concepts

After identifying the problem and opportunity, the next aspect of the study is to develop potential solutions to the problem and evaluate the solutions. Following identification of the problem and opportunity, three alternative concept options for the realignment of Ashburnham Drive were developed and presented to the public for review and feedback as part of Public Information Centre (PIC) #1 (illustrated in **Figure 1**). The options generally varied in the extent and location of the roadway realignment between Maniece Avenue, Hunter Street, and McFarlane Street, as well as in their associated intersection improvements.

Figure 1: PIC #1 Ashburnham Drive Realignment Concept Options



Each alignment was evaluated based on transportation performance, natural heritage impacts (including crossings of North Meade Creek or Golf Course Creek and floodplain effects), cultural heritage and socio-economic considerations, impacts to parkland, golf course and future development lands, potential effects on nearby residences, and proximity to the Trent–Severn Waterway. The evaluation of these options informed the identification of a preferred alignment that best balances safety, connectivity, environmental protection, and community impacts.

For PIC#2, four preliminary Alternative Design Concepts were developed based on the feedback received during and following PIC#1. The preferred alignment includes varying modifications to Ashburnham Drive's alignment connecting with Maniece Avenue, Hunter Street, Old Norwood Road, as well as new connections to existing active transportation facilities located to the north and south of the study area.

McFarlane Street Bridge Concepts

To address transportation concerns associated with the existing McFarlane Street Bridge, five concept options were developed and evaluated (illustrated in **Figure 2**). Options ranged from constructing a new bridge north or south of the existing crossing to replacing the bridge on the current alignment. The evaluation considered transportation performance and constructability, along with potential effects to the Natural Areas and Corridors near the Trent Canal, including vegetation removal and disturbance limits, and the need to manage in-water works where applicable.

Figure 2: McFarlane Street Bridge Concept Options



From a cultural heritage perspective, options that created a new crossing location introduced a new effect on a National Historic Site, while replacement on the existing alignment had the potential to directly affect an identified cultural heritage resource associated with the current bridge. Property impacts also varied by option: some alternatives presented higher potential for direct effects to nearby residences and parkland, while others were expected to avoid direct residential impacts but still required varying degrees of road realignment to tie into the new bridge location.

In developing and refining the bridge concepts, the study also considered broader possibilities for how the corridor functions, including whether the existing bridge could be retained for active transportation purposes under certain scenarios and how the new structure could best serve all users. The existing McFarlane Street bridge is owned and maintained by Parks Canada and accordingly there is limited certainty about its future maintenance and upkeep. Across the options, a new bridge configuration capable of accommodating two travel lanes and all-ages-and-abilities active transportation facilities was considered feasible, with final span arrangement, staging, and restoration commitments to be confirmed through detailed design process.

In addition to evaluating alternative road and bridge alignments, the study also considered a range of potential road and bridge cross-section options to address existing transportation deficiencies and improve multi-modal connectivity. These alternatives explored different ways of accommodating pedestrians and cyclists within the available right-of-way and reflected both current conditions and opportunities to enhance safety and comfort for all users. Collectively, these cross-section options informed the selection of a preferred design that balances transportation function, active transportation needs, corridor constraints, and overall constructability, with final dimensions and details to be confirmed during detailed design.

Recommended Design for Ashburnham Drive

The recommended design for Ashburnham Drive realigns the roadway east of the Trent–Severn Waterway, with Sidewalks and Boulevard Bicycle Lanes on both sides as illustrated in **Figures 3 and 4**. This Alternative Alignment for the Ashburnham Drive was selected as it avoids direct impacts to identified cultural heritage resources, and offers the greatest flexibility to meet design standards related to roadway geometry, sightlines, and cross-section configuration. This alignment also results in the lowest potential impacts on fish and fish habitat when compared to the other alternatives considered. In addition, the recommended design, together with the potential repurposing of the existing Ashburnham Drive corridor and City-owned lands, creates a strategic opportunity to enhance year-round public access to the Peterborough Lift Lock and the Trent–Severn Waterway National Historic Site, while supporting recreation, tourism, and broader community objectives.

Figure 2: Ashburnham Drive Recommended Design

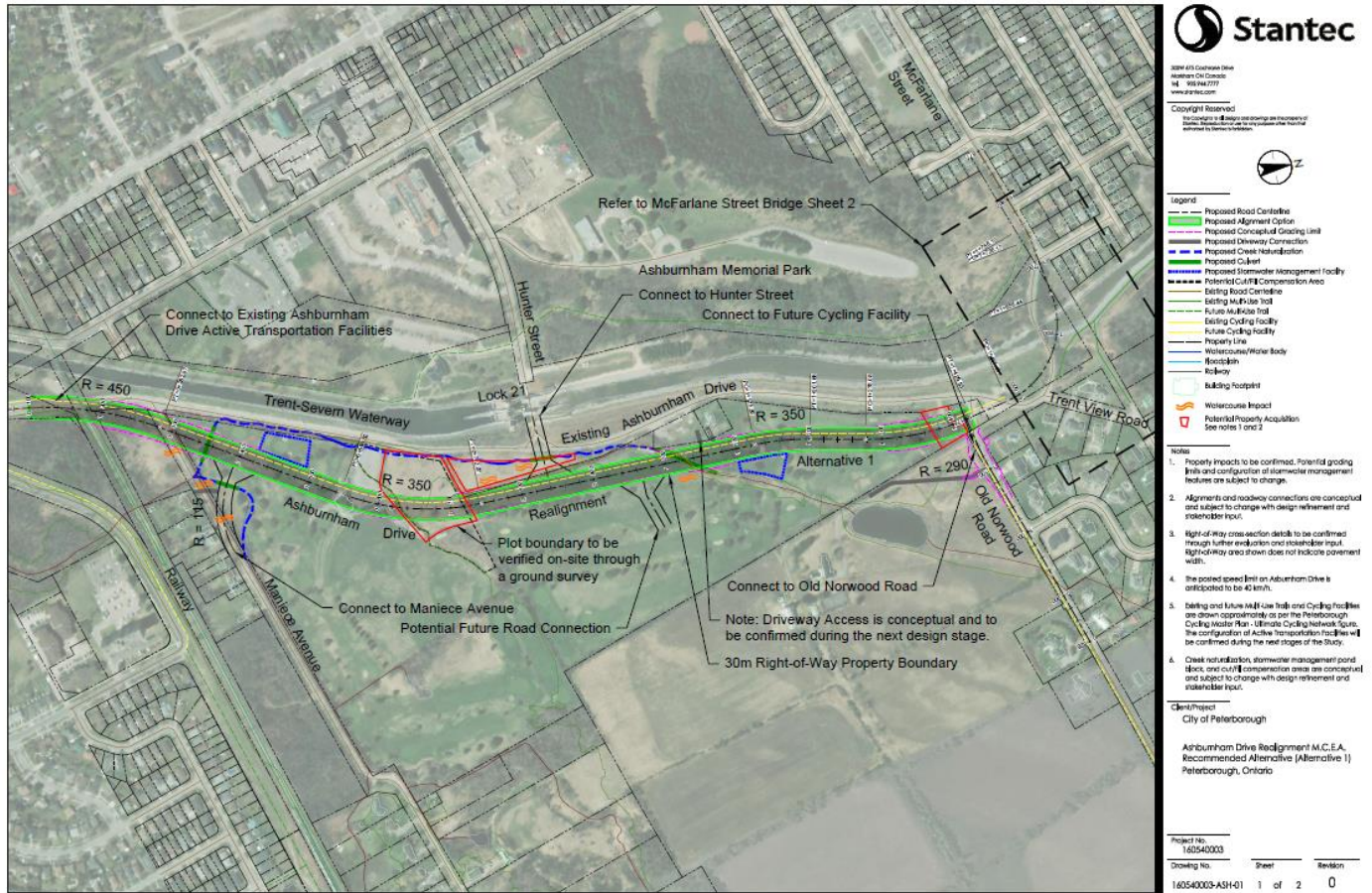


Figure 3: Ashburnham Drive Recommended Cross-Section Design



The recommended design for Ashburnham Drive has been developed in accordance with the City of Peterborough’s Engineering Design Standards and reflects the functional role of the corridor as a High-Capacity Collector with a recommended posted speed of 40 km/h. To support safe and accessible active transportation, the typical cross-section includes a 2.5 m wide boulevard, a 1.8 m wide cycling facility, a boulevard buffer, and a 1.8 m wide sidewalk. A wider 4.7 m boulevard is also incorporated to accommodate utilities, landscaping, snow storage, and to provide additional separation between the roadway and adjacent properties. North of Old Norwood Road, the right-of-

way width is reduced from 30 m to 20 m to minimize property and environmental impacts while maintaining all required roadway and active transportation elements.

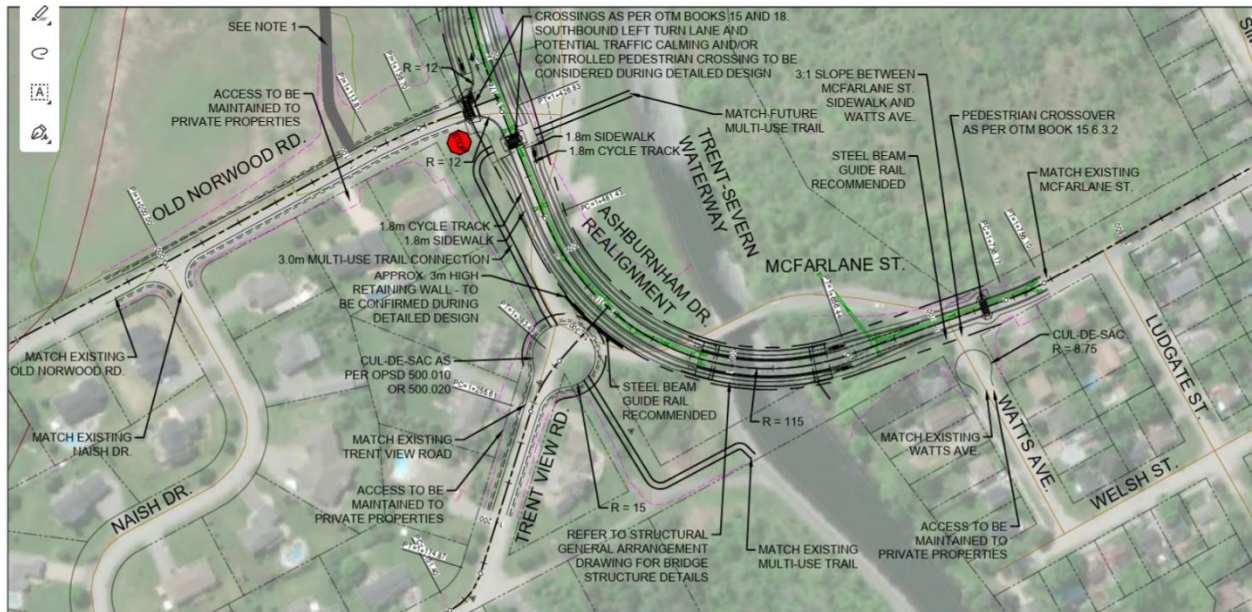
The recommended design represents the lowest relative capital cost, offers the greatest opportunity to accommodate future transit amenities, and provides fully separated facilities for pedestrians, cyclists, and vehicles on both sides of the roadway. This recommendation is contingent upon the retention of the existing Ashburnham Drive right-of-way for active transportation purposes, allowing the corridor to function safely and efficiently for all users while supporting the City's long-term transportation objectives.

Recommended Design for McFarlane Street Bridge

The recommended design for the McFarlane Street Bridge (illustrated in **Figure 5**) provides a new bridge on an alignment immediately north of the existing structure, together with a Cross-Section that incorporates Sidewalks and On-Road Cycle Lanes on both sides as illustrated in **Figure 6**. This option replaces the existing crossing with a modern structure that improves safety and functionality while maintaining continuity within the surrounding road network.

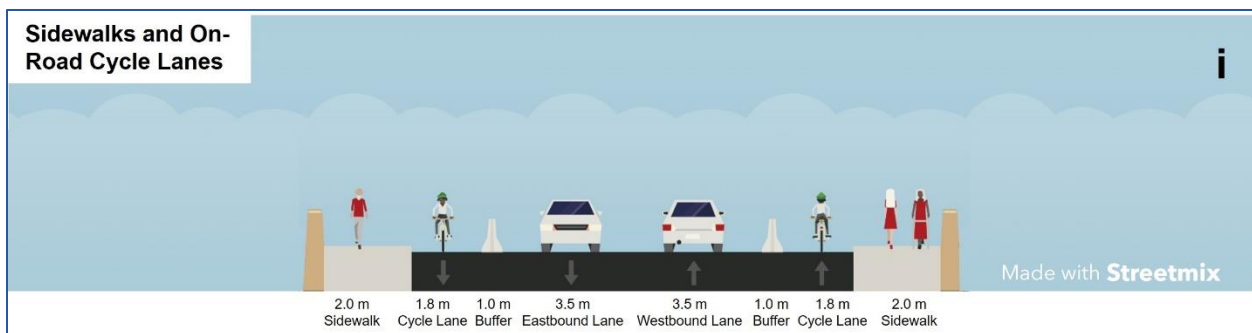
The alignment is recommended as it offers the greatest ability to achieve preferred design and operating standards, including improved horizontal and vertical geometry and a relatively short bridge span compared to other alternatives. The alignment avoids direct impacts to Ashburnham Memorial Park, is anticipated to result in lower impacts to natural features such as wildlife habitat and species at risk and provides a more straightforward and efficient construction solution. Overall, this alternative is expected to be the most cost-effective option when considering constructability, long-term performance, and lifecycle costs.

Figure 4: McFarlane Street Bridge Alignment



The Cross-Section as illustrated in **Figure 5** is recommended as it provides clear separation between all travel modes and includes dedicated pedestrian and cycling facilities on both sides of the bridge right-of-way. This bridge design option best supports and ensures long-term safe, comfortable, and accessible movement for all users and aligns with the City’s objectives for a complete, multi-modal transportation network.

Figure 5: McFarlane Street Bridge Recommended Design Cross-Section



The proposed McFarlane Street Bridge is located approximately 30 m north of the existing structure and provides a 46.7 m long, 17.2 m wide crossing of the Trent–Severn Waterway. The bridge will accommodate two traffic lanes, dedicated cycling facilities, and a sidewalk, with physical separation between travel modes to enhance safety. The structure includes exterior safety railings and has been designed to provide a minimum vertical clearance of 6.7 m above normal water levels, consistent with navigation and regulatory requirements.

Other Design Elements

The recommended design also includes two cul-de-sacs, located at Trent View Road and at the south end of Watts Avenue, to address geometric constraints and reduce impacts to adjacent properties. At Trent View Road, a cul-de-sac and associated retaining wall are proposed to avoid private property impacts that would result from maintaining a direct connection to the realigned Ashburnham Drive. At Watts Avenue, closure of the existing intersection with McFarlane Street is proposed due to roadway grade constraints, increased bridge depth, and limited ability to mitigate elevation differences resulting from the realignment. Maintaining this intersection as an at-grade connection would require extensive roadway reconstruction and significant driveway elevation changes. The proposed cul-de-sacs provide safe vehicle turning areas, maintain local access, and reduce the need for extensive roadway and property modifications. Final cul-de-sac geometry, grading, and any detailed property impacts will be confirmed during detailed design.

A detailed traffic analysis was undertaken for key intersections along the proposed Ashburnham Drive realignment to confirm turn lane requirements and traffic control under projected 2051 traffic conditions, building on the travel demand forecasts developed as part of the East Side Transportation Study. The analysis considered the feasibility of various intersection configurations, including preliminary screening for roundabouts, and assessed intersections at Old Norwood Road, the future Norman Maker Avenue, Hunter Street East, and Maniece Avenue. The intersection at Trent View Road was not assessed, as this connection is proposed to be closed as part of the recommended design.

Based on the analysis, unsignalized T-intersections with stop control and dedicated turn lanes are recommended at Old Norwood Road, Norman Maker Avenue, and Maniece Avenue, as these configurations are sufficient to accommodate future traffic demands while maintaining safe operations. Reconfiguration of the Hunter Street approach with Ashburnham Drive to provide a single shared left/right is recommended, with a signalized intersection operating under a split phasing pattern to separate traffic movements related to the single travel lane through the Lift Lock. Given that the Ashburnham corridor will have a posted maximum 40 km/h speed limit with large sections of uninterrupted traffic flow, the implementation of traffic signal controls at this location will assist in calming traffic flow.

Preliminary screening determined that roundabouts are not recommended at any of the assessed intersections due to geometric constraints, proximity to the tunnel, and potential conflicts with active transportation facilities. All intersection configurations and traffic control measures will be reviewed and confirmed during detailed design, with specific consideration given to pedestrians, cyclists, and other vulnerable road users.

A new off-road multi-use trail connection is also proposed on the east side of the realigned roadway generally between Old Norwood Road and McFarlane Avenue. New active transportation crossings of the realigned roadway would be provided near Old

Norwood Road and west of new McFarlane Street bridge to facilitate east-west and north-south active transportation movements.

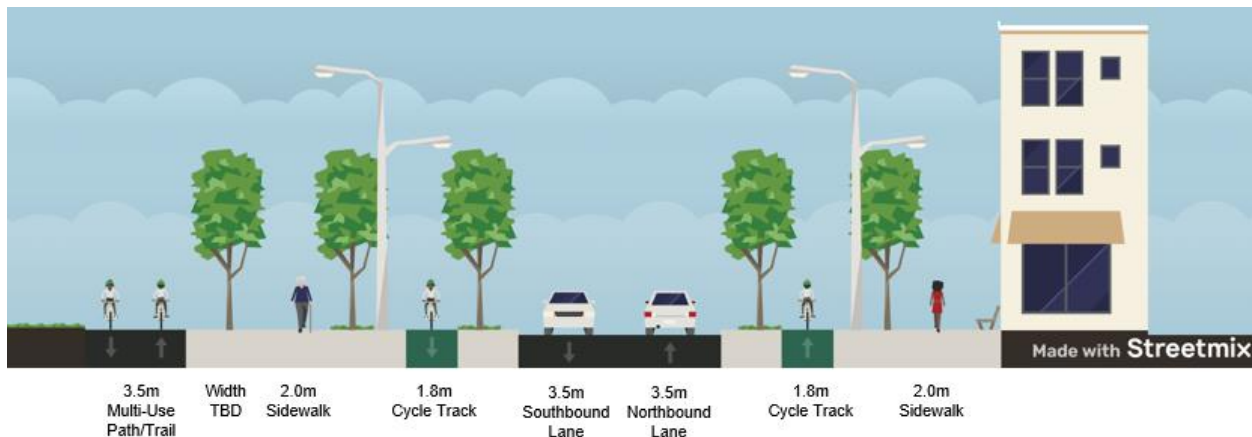
Existing Ashburnham Drive

The existing Ashburnham Drive right-of-way (ROW) and adjacent lands are located within a designated Major Open Space area that functions as a key recreational, cultural, and tourism corridor. Its proximity to the Peterborough Lift Lock, the Trent–Severn Waterway National Historic Site, and nearby destinations such as recreational trails, the Canadian Canoe Museum and Little Lake attracts residents and visitors throughout the year and supports a range of seasonal recreational and community events.

Land ownership in the area includes Parks Canada lands between the existing roadway and the waterway, privately owned lands east of the ROW, and parcels at the east limit of Hunter Street that are acquired by the City for this project. The realignment of Ashburnham Drive creates opportunities to enhance access, connectivity, and use of these lands. Potential future uses identified through the EA include retaining or repurposing the existing ROW for active transportation, enhancing the corridor with multi-use paths and wayfinding, supporting passive recreation and tourism-related amenities, exploring visitor parking opportunities, and selectively removing portions of the existing ROW to expand open space.

If the existing Ashburnham Drive ROW is not maintained or repurposed for active transportation, a modified Ashburnham Drive cross-section incorporating a multi-use path along the realigned roadway is preferred to ensure continuous and safe accommodation for pedestrians and cyclists (illustrated in **Figure 7**). The final use of the existing ROW and adjacent lands is recommended to be confirmed through a separate study during detailed design, which should be undertaken in collaboration with Parks Canada.

Figure 6: Alternate Ashburnham Drive Cross-Section with Multi-Use Path



Existing McFarlane Street Bridge

The existing bridge is owned and maintained by Parks Canada. As such there is no assurance that the bridge will remain in service of the long-term. For the purposes of this Municipal Class Environmental Assessment, it was recognized there was potential for the existing bridge to be repurposed for active transportation and passive recreational uses, and to support interpretation of the historic character of the bridge and the surrounding Trent–Severn Waterway National Historic Site. Retention of the structure could provide a dedicated crossing for pedestrians and cyclists while contributing to year-round recreational and tourism opportunities. However, this decision is not one the City is able to make as part of this EA process.

Should the existing bridge be reused, modifications would be required to meet current design and safety standards. Based on correspondence received from Parks Canada in M 2025, it is believed that Parks Canada does not intend to retain ownership of the bridge following construction of the new crossing, in which case responsibility for rehabilitation, long-term operation, and maintenance could transfer to the City if the City chooses to assume ownership of this infrastructure. Upon completion of the EA, staff will engage with Parks Canada to consider a potential change in ownership. Recognizing the future of the existing bridge may be beyond the City's control, it is therefore critical the proposed McFarlane Street Bridge include dedicated active transportation facilities.

Impacts on Properties

Construction of the preferred design will require the acquisition of three private properties, consisting of one residential property and two vacant parcels of land, and will bisect the existing Liftlock Golf Course and/or Phase 2 lands currently being planned for redevelopment. The City has reached an agreement in principle with the owner of the affected residential property that allows for acquisition of the property at a future time when the owner is ready to sell. In addition, the City has recently acquired the two vacant properties required to support the project.

In total, approximately 3.58 ha of private property will be required to accommodate the project, of which approximately 3.16 ha is associated with lands planned for redevelopment as part of the Liftlock Golf Course. The balance of property required for the new road right-of-way is currently owned and managed by Parks Canada (approximately 0.734 ha of lands within and adjacent to the Trent–Severn Waterway) and the City of Peterborough (approximately 0.87 ha of existing right-of-way).

Strategic Plan

Strategic Pillar: Infrastructure

Strategic Priority: Invest in infrastructure to ensure that it meets the future needs of our growing City.

By endorsing the recommendations of the EA and directing staff to seek Provincial approval, staff will be able to proceed, subject to future budget approval, with detailed engineering for the Ashburnham Drive Realignment and McFarlane Street Bridge replacement. This ongoing forward-looking approach to infrastructure growth in relation to population growth will position the City to continue providing high quality transportation choices.

Engagement and Consultation

Public, stakeholder, and agency engagement was a central component of the Municipal Class Environmental Assessment (MCEA) for the Ashburnham Drive Realignment and McFarlane Street Bridge Replacement project. Consultation was undertaken continuously throughout the study to ensure transparency, provide meaningful opportunities for input, and inform the evaluation and refinement of alternatives. Engagement included two Public Information Centres (PICs), direct meetings with affected property owners, extensive agency and stakeholder coordination, and targeted consultation with Indigenous communities.

Public Information Centres (PICs)

Two Public Information Centres were held to inform the public and gather feedback at key milestones in the study. PIC #1, held in September 2022, introduced the study background, the East Side Transportation Study recommendations, the MCEA process, existing conditions, evaluation criteria, and preliminary road and bridge alignment and cross-section options. PIC #1 was delivered through an online presentation and two virtual meetings to maximize accessibility. The online materials received over 2,100 visits, and the virtual sessions were attended by community members with additional feedback submitted through surveys, emails, and comment forms.

Feedback from PIC #1 focused on potential impacts to cultural heritage resources, the natural environment and floodplain, traffic operations and safety, and preferences for specific alignment and cross-section options. This input was documented, reviewed, and used to refine alternatives and evaluation criteria as the study advanced.

PIC #2, held between November and December 2023, presented the evaluation of alternative design concepts, the preliminary recommended design, anticipated environmental impacts and mitigation measures, and next steps. PIC #2 generated strong engagement, with over 2,300 visits to the online materials and a significantly

higher volume of feedback. Comments and questions addressed traffic safety, active transportation, environmental and cultural heritage considerations, property impacts, and the preliminary recommended design. The study team documented all comments received and provided responses outlining how concerns were addressed or would be considered during detailed design.

Property Owner Engagement

Recognizing the importance of minimizing impacts to private property, the City and study team conducted individual meetings with directly affected property owners in fall 2023. These meetings provided an opportunity for detailed discussion of property-specific concerns, including access, proximity of the roadway, and long-term impacts. In response to this feedback, refinements were made to the recommended design to reduce property impacts where feasible. The updated design and a written summary of changes were shared with property owners in December 2024, and feedback indicated general support for the refinements. The City will continue to work closely with affected owners during detailed design to confirm impacts and mitigation measures.

Agency and Stakeholder Consultation

A wide range of federal, provincial, municipal, and community agencies were consulted throughout the study. Agencies were engaged at key milestones through notices, correspondence, meetings, and review of technical materials to ensure regulatory requirements and technical considerations were addressed. This included ongoing coordination with emergency services, school board, transportation agencies, and conservation authority.

A Technical Advisory Committee (TAC) comprising City staff and representatives from key agencies was established at the outset of the study and met at critical stages to provide technical input, review alternatives, and confirm project requirements.

Key Agency Coordination

Parks Canada was a primary stakeholder due to the project's proximity to the Trent–Severn Waterway National Historic Site and the Peterborough Lift Lock. Three formal meetings were held with Parks Canada to review existing conditions, alignment and cross-section options, cultural heritage considerations, and design refinements. Parks Canada provided direction on canal infrastructure protection, bridge setbacks, heritage conservation, and long-term operational considerations, all of which informed the preferred design.

Otonabee Conservation was also closely involved, with multiple meetings held to address floodplain constraints, stormwater management, and hydraulic considerations. Input from Otonabee Conservation guided the development of the stormwater management strategy and confirmed regulatory requirements.

Indigenous Consultation

Indigenous consultation was undertaken in accordance with provincial and federal guidance. Notifications and invitations to consult were issued to all Williams Treaties First Nations and Métis organizations with asserted interests in the study area. Meetings were held with Curve Lake First Nation and Hiawatha First Nation to share study information, review technical findings, and gather feedback on cultural heritage, archaeological resources, natural heritage, and species at risk. Concerns raised particularly regarding wetlands, species at risk, and archaeological resources were addressed through additional technical studies, commitments for further investigation during detailed design, and continued engagement.

Summary of Key Themes and Responses

Across all engagement activities, consistent themes emerged, including:

- The importance of protecting cultural heritage resources and the canal corridor;
- Strong support for enhanced and separated active transportation facilities;
- Concerns regarding traffic safety and future growth;
- Interest in repurposing the existing Ashburnham Drive corridor for recreation and open space;
- Sensitivity to property impacts and construction staging.

The recommended road alignment and bridge design reflect these themes and demonstrate how public, stakeholder, and agency input directly influenced the project outcome. A detailed record of comments and responses is provided in the ESR appendices.

Budget and Financial Implications

The Environmental Assessment (EA) includes preliminary, order-of-magnitude cost estimates for both the recommended road realignment works and the replacement of the McFarlane Street bridge. All cost estimates are expressed in 2025 dollars and are intended for planning purposes only. Costs will be reviewed and refined during the detailed design phase.

The preliminary, order-of-magnitude capital cost for the recommended road realignment and McFarlane Street bridge replacement is estimated at approximately \$27.8 million (2025 dollars).

This includes approximately \$17.3 million for the recommended road alignment and associated works (inclusive of a 30 per cent contingency) and approximately \$10.5 million for the replacement of the McFarlane Street bridge, including associated retaining walls and a 30 per cent contingency.

These preliminary estimates do not include property acquisition costs or costs associated with municipal servicing and new utilities (e.g., hydro, telecommunications, gas). All cost estimates are for planning purposes only and will be reviewed and confirmed during the detailed design phase.

Approval of the recommendations in this report has no immediate financial implications. Any costs associated with detailed design, property acquisition, and construction will be subject to future capital budget approvals.

Conclusion

The Ashburnham Drive Realignment and McFarlane Street Bridge Replacement Municipal Class Environmental Assessment represents a comprehensive and collaborative planning effort that responds directly to community priorities and the City's long-term transportation and infrastructure objectives. Through extensive consultation with residents, affected property owners, agencies, Indigenous communities, and key stakeholders, the study carefully evaluated alternatives and refined the preferred solution to improve safety, enhance active transportation, protect cultural and natural heritage, and minimize impacts to private property. The recommended design reflects a balanced approach that supports reliable transportation infrastructure while respecting the unique setting of the Trent–Severn Waterway National Historic Site and the Peterborough Lift Lock.

Approval of the Environmental Assessment recommendations will allow the City to formally complete the MCEA process and advance the project to detailed design, where further refinement, permitting, and continued engagement will occur. The City's Transportation Master Plan (2022) identifies this project as a medium-term improvement, with construction currently anticipated within a 10- to 20-year horizon, subject to future Council direction and capital budget approvals.

Approval at this stage does not commit the City to immediate construction, but establishes a clear, Council-endorsed framework to guide future planning, funding decisions, and implementation.

Attachments

Appendix A: Recommended Design for Ashburnham Drive Realignment and
McFarlane Street Bridge Replacement

Submitted by,

Blair Nelson, BEng., P.Eng.
Commissioner, Infrastructure, Planning and Growth Management

Contact Name:

Ian Boland, C.E.T.
Director, Asset Management and Capital Planning (Acting)
Phone: 705-742-7777 Ext. 1504
Toll Free: 1-855-738-3755
Email: iboland@peterborough.ca

Robert Dunford
Manager, Transportation Planning
Phone: 705-742-7777 Ext. 1867
Toll Free: 1-855-738-3755
Email: rdunford@peterborough.ca

Vinod Soman
Senior Project Manager- Transportation
Phone: 705-742-7777 Ext. 1749
Toll Free: 1-855-738-3755 Ext. 1749
Email: vsoman@peterborough.ca